

July 26, 2022

Carla Nixon, Town Planner  
Town of Cumberland  
290 Tuttle Road  
Cumberland, ME 04021

**Re: Wyatt Drive – Proposed Condominium  
Sketch Plan Review of Alternatives**

Dear Carla and Members of the Planning Board,

On behalf of Maine Affordable Properties, LLC, (MAP) please find attached sketch plans and supportive documentation for a new residential neighborhood in the town center. The property is in the Town Center District (TCD) and Setback Overlay 2 (SB2). The property is shown on Tax Map U11, Lot 4A and is 2.55 acres total or 111,151 square feet. The existing site is currently undeveloped and primarily open field meadow with a mix of mature evergreen and deciduous trees along the property lines.

As you may recall, the previous conceptual plan for this development was for a traditional subdivision design of 7 single-family homes on individual lots. That plan is also included with this submission but is not the plan preferred by the applicant. Following what we heard at that initial meeting and further consideration of the town's goals, ordinances and design standards, the applicant felt the most beneficial design for town center would be smaller, more affordable homes. The proposed plan is now for 9 duplex buildings or an 18-unit condominium neighborhood. This use is allowed in the TCD District but shall comply with the TCD Design Guidelines. Although there is only one lot, a condominium development is considered a subdivision and shall comply with the Subdivision Ordinance based on the clustered subdivision standards

The TCD Design Standards were adopted in 2011 and contain a list of standards that all new development must comply with, e.g., architecture (in particular, New England style architecture), construction of buildings closer to the roads, new street trees, minimal signage and minimal lighting.

The proposed duplex site plan shows an efficient, compact and low impact neighborhood design with an 18' wide road and 22' deep driveways to minimize impervious surfaces and stormwater runoff. The compact design minimizes removal of existing wooded buffer along the property lines.

Per Ord. Sect. 315-21.d: In the case of duplex or multiplex developments, the minimum lot area per dwelling unit shall be no less than 5,000 square feet if on public sewer, which allows 19 units

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22 Abby Lane  
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on this site. We are proposing 18. At this point we are still working through the details of what the buildings and final site plan will look like but our preliminary plan shows 18 units of smaller, affordable homes within 9 duplex buildings.

## DESIGN SUMMARY

**Homes.** We are proposing 8 new 2-story duplex structures with a footprint of 26' x 74' each. The new homes will be very modest in size with a footprint of 23' x 26' each (approximately 1,200 s.f each). Each unit will have a one-car garage. An additional parking space will be provided in the driveway of each unit. Additional parking areas will be provided for visiting guests. Each unit will have a back yard patio of approximately 12' x 16'.

**Road.** The proposed new road is an 18' wide, private road, to be owned and maintained by the new Homeowners Association. We have incorporated a T-Shaped turnaround at the end of the road to allow for internal emergency access and snow plow or deliver van turnaround.

**Stormwater.** Stormwater is proposed to be managed as much as possible by low-impact design; by the use of foundation perimeter drip edge infiltration and by small bio-engineered stormwater basins, also known as a raingardens. The raingarden is long and shallow and will be planted with appropriate shrubs and perennials to help filter and absorb runoff. The raingarden will be a visually appealing asset to the development.

**Landscaping.** Maine Affordable Properties is committed to providing appropriate screening and buffering around the perimeter of the property to satisfy abutter concerns. Additionally, street trees and front and back yard landscaping for each unit will be provided as part of the beauty of condominium developments. Anderson Landscaping will be responsible for the installation of the proposed landscaping.

Professionals involved with the preparation and design of this project include:

**Landscape Architect** - Tom Farmer, Landscape Architect will be designing the new neighborhood, ensuring compliance with the local ordinances and guidelines and representing MAP through the Planning Board process.

**Surveying** – Boundary Point Land Surveying, LLC, has provided boundary and topographic survey.

**Civil Engineering** - Trillium Engineering Group is providing stormwater design and road engineering.

Along with this cover letter and ordinance narrative, this submission included 5 copies of the following information:

- L-1 Existing Conditions Plan overlaid on a Google Earth aerial image, showing the site surrounding context.
- L-2 Proposed Duplex Condominium Plan
- L-3 Alternative 7-lot Traditional Subdivision Plan
- A-1 Proposed Duplex Architectural Building Elevations
- A-2 Proposed Duplex Architectural Floor Plans
- P-1 Context Pictures of the site and surrounding context

We look forward to discussing our plans and scheduling a site walk with the Board at their August 16, 2022 meeting. Should you have any questions or need additional information prior to that, please do not hesitate to contact me.

Thank you,



Tom Farmer  
Landscape Architect

## ORDINANCE NARRATIVE

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The following text shown in *italics* is copied directly from the Ordinance, outlining various codes and submission requirements. Our responses follow each item and are shown **indented in normal bold font**.

*NET RESIDENTIAL ACREAGE — Net residential acreage shall be determined by subtracting from gross acreage available the following:*

- A. *Area for roads and parking as shown on the proposed plan.*

**11,952 s.f. (610' road x 18' wide + 6 parking spaces)**

- B. *Land which is cut off from the main parcel by a road, existing land uses, a utility easement or right-of-way or major stream so as to serve as a major barrier to common use or so that it is isolated and unavailable for building purposes (final determination by Planning Board).*

**None**

- C. *Other areas which are difficult to develop in their natural state because of topography, drainage, or subsoil conditions. Specific conditions include but are not limited to:*

*(1) Slopes in excess of 20% sustained for 30,000 square feet or more.*

**None**

*(2) Wetlands as defined in the Federal Manual for Identifying and Delineating Jurisdictional Wetlands dated January 10, 1989, and as amended from time to time.*

**None**

*(3) Land shown to be in a one-hundred-year flood zone as shown on the Flood Insurance Rate Map (FIRM).*

**None**

- D. *Land in rights-of-way or easements, but not including land in open space easements*

**None**

- E. *Resource Protection Districts*

**None**

**NET RESIDENTIAL ACREAGE: 111,151 s.f. (gross) - 11,952 s.f. (roads & parking) = 99,199 s.f.**

## **NET RESIDENTIAL DENSITY**

*Per Sect. 315-21.d, the following lot standards shall apply within the TCD:*

- (1) Minimum lot size of 10,000 square feet per unit if on public sewer; 20,000 square feet if not on public sewer.*
- (2) In the case of duplex or multiplex developments, the minimum lot area per dwelling unit shall be no less than 5,000 square feet if on public sewer.*

**This parcel is served by public sewer and water, so the minimum lot area per dwelling unit shall be no less than 5,000 s.f.**

**NRA of 99,199 s.f. / 5,000 s.f. = 19.8 or 19 Units maximum net residential density.**

## **COMMON OPEN SPACE**

Per Sect. 250-11, at least 25% of the total area of the tract or parcel of land being developed as a Clustered Subdivision must be maintained as common open space and not included in the individual building lots.

25% of the total 111,151 gross s.f. of land equals 27,788 s.f. of required open space. We are proposing the 30' perimeter buffer (20' on cemetery side) as the proposed open space. The total area of land in the perimeter buffer is approximately 35,000 s.f. The perimeter buffer will be protected in a deed restricted "no-cut, no disturbance buffer". The open space buffer will be owned, protected and maintained by the Homeowners Association (HOA), via covenants and restrictions outlined in the HOA Protective Covenants for the subdivision.

## **TECHNICAL AND FINANCIAL CAPACITY**

One of the partners of Maine Affordable Properties, Robert Anderson owns Anderson Landscaping Company in North Yarmouth as well as Toddy Brook Golf Course. Anderson Landscaping is a well-established landscape construction company that has been in business for over 30 years, providing excavation, site work, landscape construction and maintenance. Anderson Landscaping of North Yarmouth and Maine Affordable Properties brings their combined experience to this project. The applicant has significant experience in neighborhood and residential home development. They are both owners and site and building contractors which will guarantee a high-quality final product. Some of their recent projects include:

- Eagle Ridge Condominiums, original owner, site contractor and developer for a 14-unit condominium development, North Yarmouth, ME

- Knights Way Subdivision, original owner, site contractor and developer for a 9-lot single family residential neighborhood, North Yarmouth, ME
- Pinehurst Residential Subdivision; original owner and site contractor for a 5-lot single family residential neighborhood, North Yarmouth, ME
- Spring Street Condominiums; a 10-unit duplex neighborhood in Gardiner, ME
- Toddy Brook Golf Course. A premier, 18-hole Golf Course on 140 acres, including a driving range and club house, restaurant, pro shop and a 110-car parking lot. Completely built, maintained and still owned by Robert Anderson.

Maine Affordable Properties, via a credit extension with Norway Savings Bank will be the primary financing for the project. A bank statement will be provided once we get through the Preliminary Subdivision phase and have a better understanding of total project costs and financial needs.

## **BUILDING SETBACKS**

*Per sect. 315-22 Setback Overlay Districts, there are no setback provisions for the Town Center District.*

*Per Sect. 315-21.d, the following minimum setbacks are required for all structures in the TCD, except that sheds and driveways are permitted to a minimum setback of eight feet from the side and rear lot lines:*

- (1) *Front: 15 feet.*
- (2) *Rear: 15 feet.*
- (3) *Side: 10 feet*

## **REQUESTED WAIVERS**

**75' Perimeter Buffer:** Although this duplex condominium development is not technically a “clustered” subdivision we are using the clustered subdivision standards in our approach. One of those standards includes the requirement for a 75' perimeter buffer or setback from property line and new structures. Conversely, the Town Center District setbacks only require 15' front and rear and 10' side. Given the size and shape of this lot, a 75' buffer would consume approximately 2 of the 2.55 acres.

The property is bordered by the school campus to the west, which has an 8' high solid vinyl fence, the Congregational Cemetery on Main Street to the south, an apartment building, Dara Bistro and a single-family home to the east, and two single family homes to the north. After careful consideration, the proposed design for the duplex development maintains a minimum of 30' structure setback and more where possible. In comparison, the setbacks for the 6-unit condominium development at Village Way has 23' structure setbacks in the rear of the units. To help with the buffering of noise and views for the 2 homes on either side of the new entrance

drive, the driveway will be buffered with a continuous line of understory shrubs and street trees at 60' on center.

**Road width:** Per sect 250-37, Table 1 General Street Standards. A Duplex, condominium development generates 8 trips per unit or 144 trips per day from an 18-unit development. The 144 trips per day would require us to build a road according to the Residential Access Street Standards at 22' paved width. We are asking for a waiver to allow us to construct our road to the Residential Private Street Standards of 18' paved width.

*Private streets serve as feeders to access, subcollector, and collector residential streets and may be the principal entrance streets of a residential subdivision.  
Private streets are permitted only when the average daily trips are less than 50.*

We feel this is an acceptable request, given that the road is a dead-end road, only 620' long and the homes are compact, close together with small driveways. Traffic speeds will be very low, well below the 25 MPH design speed for these roads. Less impervious surface will also lead to a lower-impact stormwater design and treatment plan.

**A high-intensity soil survey.** *A high-intensity soil survey shall be conducted by a certified soil scientist to identify soils within the proposed development in accordance with United States Department of Agriculture Natural Resources Conservation Service National Cooperative Soil Classification. The soil boundaries and names shall be superimposed on a plot plan of the proposed development.*

Typically, a high-intensity soils survey is necessary and useful on larger development projects, where there are a variety of soils. The survey helps to identify where potentially erodible soils are, high water tables, potential locations for subsurface septic disposal fields could be located, potentially unsuitable / suitable soils for building and foundation placement, roadways, etc.

We do not feel there are any concerns for dangerous or unhealthy conditions resulting from the lack of a high-intensity soils report. We are seeking a waiver of this requirement for the following reasons:

- The project site is small at 2.5 acre,
- There are no streams or wetlands on or near the property,
- The site is already surrounded by similar development with supporting soils,
- We are not proposing on-site wells or septic fields,
- The site is almost perfectly flat with less than 1% slope so there are no unstable or erodible soils to be identified nor a concern of soil erosion or negative stormwater runoff impacts to abutting properties,

**From:** Daniel Diffin <[dpd@smemaine.com](mailto:dpd@smemaine.com)>  
**Sent:** Thursday, August 11, 2022 2:13 PM  
**To:** Carla Nixon <[cnixon@cumberlandmaine.com](mailto:cnixon@cumberlandmaine.com)>  
**Cc:** Tom Farmer <[tsffarmer@gmail.com](mailto:tsffarmer@gmail.com)>  
**Subject:** RE: Wyatt Drive Subdivision digital submission

**WARNING:** This is an external email that originated outside of our email system. DO NOT CLICK links or open attachments unless you recognize the sender and know that the content is safe!

Hi Carla,

Please see below for some initial comments on the Sketch Plan Submission for the Wyatt Drive Subdivision:

1. We have reviewed the waiver requests and find the following:
  - a. 75' Perimeter Buffer – SME recommends approval of this waiver.
  - b. Road Width – With the proposed 18-units, the projected vehicles per day would be 144 per Table 1 of Section 250-37 of the Subdivision Ordinance, which is well above the maximum 50 trips per day limit for Private Street Standards. As noted in the Cover Letter provided, this level of use would classify the subdivision street as a Residential Access Street with a required width of 22-feet of pavement with additional 2-foot shoulders on each side. SME recommends that the Applicant provide further information as to why a 22-foot-wide section would not work on this site prior to further consideration of this waiver request. With that, SME recommends a detailed analysis of the required turning maneuvers for emergency vehicles to demonstrate that the proposed road design is adequate.
  - c. High-Intensity Soil Survey – SME recommends approval of this waiver.
2. The Net Residential Acreage Calculation should be adjusted based on the final width of the roadway.
3. We recommend that the Applicant confirm the sewer main elevations in Tuttle Road early in the design process. In recent Town projects, it was found that this line was generally shallow, and the Applicant may need to consider connecting with the existing sewer easement to the north of the project site.
4. We recommend that the Applicant coordinate with the homeowner directly north of the intersection with Tuttle Road. It appears that a portion of the abutter's driveway is on the property required for the connection to Tuttle Road.

These are some preliminary thoughts on the information provided. Please don't hesitate to reach out to me to discuss in more detail.

Thanks,

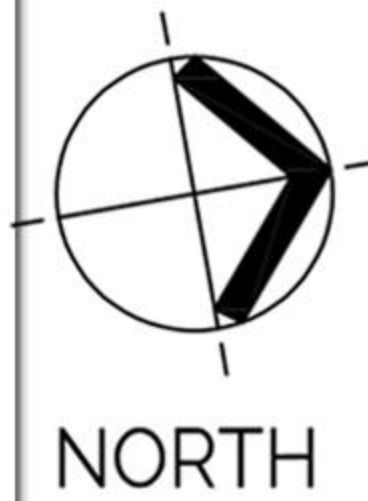
Dan

Daniel P. Diffin, P.E., LEED AP BD+C  
*Vice President/Senior Civil Engineer*

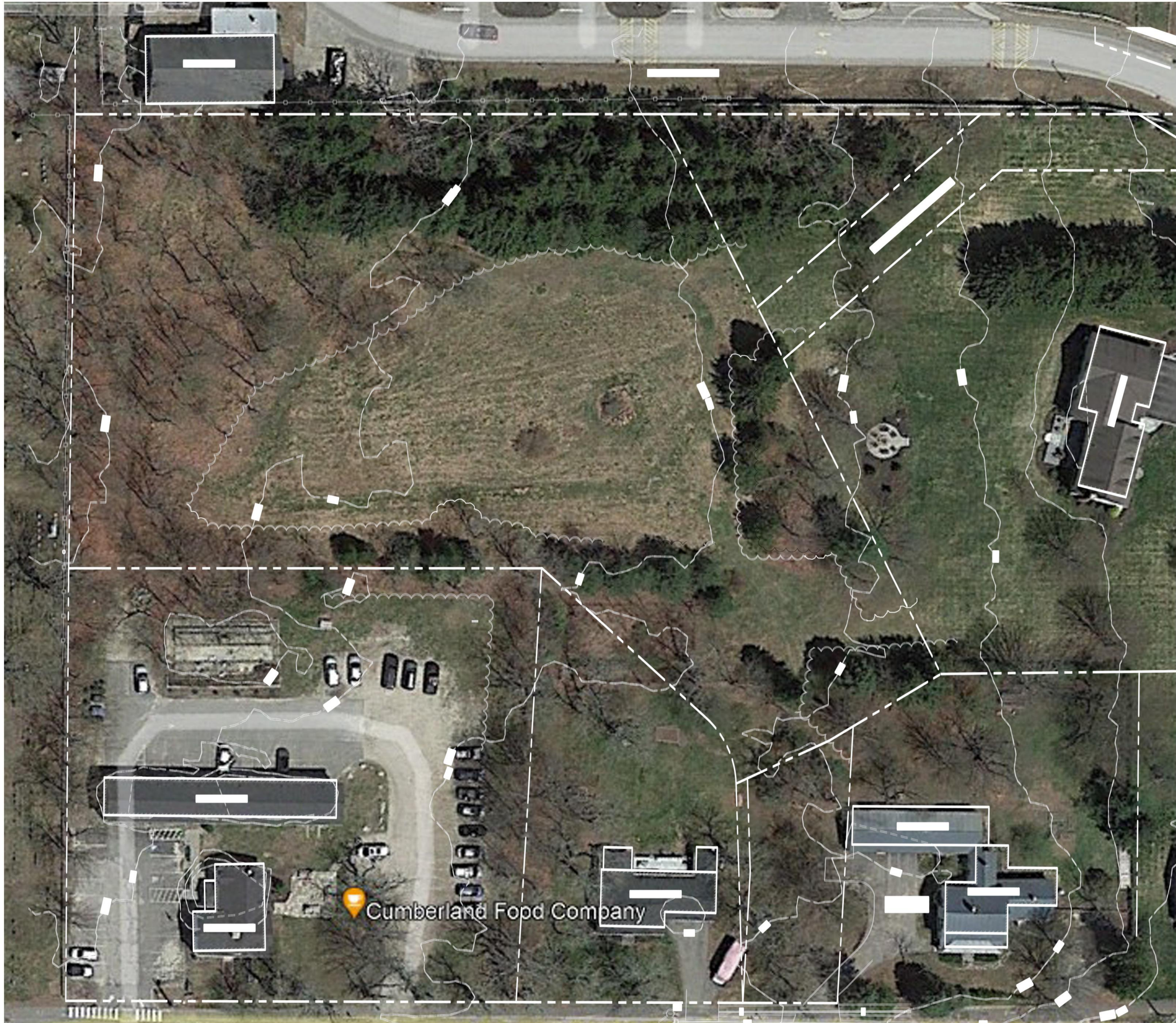


**Sevee & Maher Engineers, Inc.**  
4 Blanchard Road, P.O. Box 85A  
Cumberland, ME 04021  
Office: 207.829.5016  
Cell: 207.240.3315  
Fax: 207.829.5692

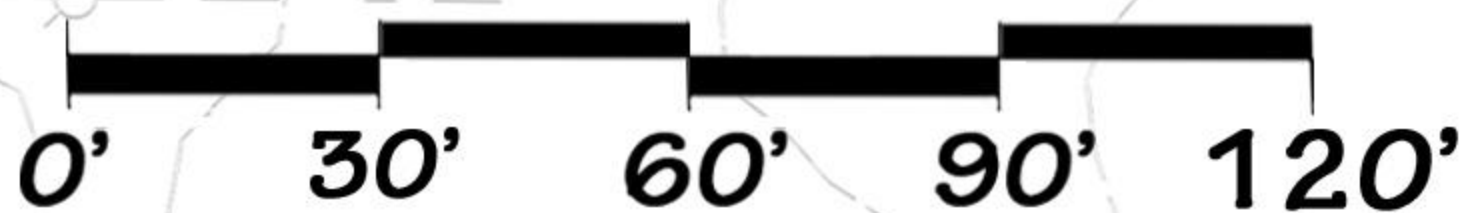




NORTH



GRAPHIC SCALE: 1" = 60' @ 11" X 17" sheet



GENERAL NOTES

- 1) BOUNDARY AND TOPOGRAPHY INFO. TAKEN FROM "EXISTING CONDITIONS SURVEY FOR WILLIAM D. WYATT OF PROPERTY ADJOINING 363 TUTTLE ROAD, CUMBERLAND, MAINE" PREPARED BY BOUNDARY POINTS PROFESSIONAL LAND SURVEYING, LLC DATED SEPTEMBER 21, 2018
- 2) THE CONTRACT WORK TO BE PERFORMED ON THIS PROJECT CONSISTS OF FURNISHING ALL REQUIRED LABOR, MATERIALS, EQUIPMENT, IMPLEMENTS, PARTS AND SUPPLIES NECESSARY FOR OR APPURTENANT TO, THE INSTALLATION OF CONSTRUCTION IMPROVEMENTS IN ACCORDANCE WITH THESE DRAWINGS AND AS FURTHER ELABORATED IN ANY ACCOMPANYING SPECIFICATIONS.
- 3) THE WORK SHALL BE PERFORMED IN A THOROUGH WORKMANLIKE MANNER. ALL CONTRACTORS TO CONFORM TO ALL APPLICABLE OSHA STANDARDS. ANY REFERENCE TO A SPECIFICATION OR DESIGNATION OF THE AMERICAN SOCIETY FOR TESTING MATERIALS, FEDERAL SPECIFICATIONS, OR OTHER STANDARDS, CODES OR ORDERS, REFERS TO THE MOST RECENT OR LATEST SPECIFICATION OR DESIGNATION.
- 4) ALL CONSTRUCTION WITHIN THE TOWN RIGHT OF WAY SHALL COMPLY WITH TOWN PUBLIC WORKS STANDARDS. ALL CONSTRUCTION WITHIN A STATE RIGHT OF WAY SHALL COMPLY WITH MAINE D.O.T. STANDARDS. ALL UTILITY CONSTRUCTION SHALL CONFORM TO RESPECTIVE UTILITY STANDARDS.
- 5) THE OWNER IS RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS REQUIRED BY THE TOWN PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS FROM THE TOWN REQUIRED TO PERFORM ALL THE WORK (STREET OPENINGS, BUILDING PERMIT, ETC.). THE CONTRACTOR SHALL POST ALL BONDS AS REQUIRED, PAY ALL FEES, PROVIDE PROOF OF INSURANCE AND PROVIDE TRAFFIC CONTROL NECESSARY FOR THIS WORK.
- 6) PRIOR TO CONSTRUCTION, THE SITE CONTRACTOR IS TO INFORM ALL AREA UTILITY COMPANIES AND GOVERNMENTAL AGENCIES OF PLANNED CONSTRUCTION. THE SITE CONTRACTOR IS REQUIRED TO CONTACT DIG-SAFE (1-800-225-4977) AT LEAST 3 BUSINESS DAYS PRIOR TO ANY EXCAVATION TO VERIFY ALL UNDERGROUND AND OVERHEAD UTILITY LOCATIONS.
- 7) THE PROJECT DRAWINGS ARE GENERALLY SCHEMATIC AND INDICATE THE POSSIBLE LOCATION OF EXISTING UNDERGROUND UTILITIES. INFORMATION ON EXISTING UTILITIES HAS BEEN COMPILED FROM AVAILABLE INFORMATION INCLUDING UTILITY COMPANY MAPS, MUNICIPAL RECORD MAPS, AND FIELD SURVEY. IT IS NOT GUARANTEED TO BE CORRECT OR COMPLETE. UTILITIES ARE SHOWN TO ALERT THE CONTRACTOR TO THEIR PRESENCE. THE CONTRACTOR IS SOLELY RESPONSIBLE FOR DETERMINING ACTUAL LOCATIONS AND ELEVATIONS OF ALL UTILITIES, INCLUDING SERVICES, WHEN THOSE SERVICES ARE TO BE LEFT IN PLACE. THE CONTRACTOR IS TO PROVIDE ADEQUATE MEANS OF SUPPORT AND PROTECTION DURING THE EXCAVATING AND BACKFILLING OPERATIONS. SHOULD ANY UNCHARTED OR INCORRECTLY CHARTED UTILITIES BE FOUND, THE CONTRACTOR SHALL CONTACT THE DESIGN ENGINEER IMMEDIATELY FOR DIRECTIONS BEFORE PROCEEDING FURTHER WITH THE WORK IN THIS AREA.
- 8) OSHA REGULATIONS MAKE IT UNLAWFUL TO OPERATE CRANES, BOOMS, HOISTS, ETC. WITHIN TEN FEET (10') OF ANY ELECTRIC LINE. IF THE CONTRACTOR MUST OPERATE CLOSER THAN 10', THE CONTRACTOR MUST CONTACT THE POWER COMPANY TO MAKE ARRANGEMENTS FOR PROPER SAFEGUARDS BEFORE ENCROACHING ON THIS REQUIREMENT.
- 9) IT IS THE CONTRACTOR'S RESPONSIBILITY TO EXAMINE ALL PLANS, APPROVALS, AND DETAILS FOR ADDITIONAL INFORMATION. THE CONTRACTOR SHALL VERIFY ALL THE SITE CONDITIONS IN THE FIELD AND CONTACT THE DESIGN ENGINEER IF THERE ARE ANY DISCREPANCIES REGARDING THE CONSTRUCTION DOCUMENTS AND/OR FIELD CONDITIONS SO THAT AN APPROPRIATE REVISION CAN BE MADE PRIOR TO BIDDING.
- 10) THE CONTRACTOR SHALL REFERENCE ARCHITECTURAL PLANS FOR EXACT DIMENSIONS AND CONSTRUCTION DETAILS OF THE BUILDING AREA. BUILDING AND DRIVEWAYS SHOWN ARE CONCEPTUAL. ALL SITE DIMENSIONS ARE REFERENCED TO PROPERTY LINES, THE FACE OF CURBS, OUTSIDE FACE OF WALLS, OR EDGE OF PAVING UNLESS OTHERWISE NOTED.
- 11) ALTERNATIVE METHODS AND PRODUCTS OTHER THAN THOSE SPECIFIED MAY BE USED IF REVIEWED AND APPROVED IN WRITING BY THE OWNER, DESIGN ENGINEER, AND APPROPRIATE GOVERNMENTAL AGENCY PRIOR TO INSTALLATION.
- 12) THE CONTRACTOR SHALL RESTORE ALL UTILITY STRUCTURES, PIPE, UTILITIES, PAVEMENT, CURBS, SIDEWALKS, AND LANDSCAPED AREAS DISTURBED BY CONSTRUCTION TO AS GOOD AS BEFORE BEING DISTURBED AS DETERMINED BY CITY CODE ENFORCEMENT OFFICIALS. ANY DAMAGES SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- 13) ALL EXCAVATION SHALL BE BACKFILLED TO EXISTING GRADE BEFORE THE END OF THE DAY OR ADEQUATELY PROTECTED FROM DANGER TO HUMANS AND ANIMALS.
- 14) THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING ALL FIELD LAYOUT. THE OWNER WILL PROVIDE A BENCH MARK AT THE CONSTRUCTION SITE FROM WHICH TO BEGIN LAYOUT.
- 15) THE CONTRACTOR SHALL GUARANTEE THE FAITHFUL REMEDY OF ANY DEFECTS DUE TO FAULTY MATERIALS OR WORKMANSHIP AND GUARANTEES PAYMENT FOR ANY RESULTING DAMAGE WHICH SHALL APPEAR WITHIN A PERIOD OF ONE (1) YEAR FROM THE DATE OF SUBSTANTIAL COMPLETION OF THE PROJECT.
- 16) THE CONTRACTOR SHALL PROVIDE AS-BUILT RECORDS OF ALL CONSTRUCTION (INCLUDING UNDERGROUND UTILITIES) TO THE OWNER AT THE END OF CONSTRUCTION.
- 17) A PRE-CONSTRUCTION CONFERENCE WITH THE OWNER, DESIGNERS, TOWN OFFICIALS AND CONTRACTOR SHALL BE REQUIRED BEFORE ANY CONSTRUCTION OCCURS ON THE PROJECT. DURING CONSTRUCTION THERE SHALL BE WEEKLY PROGRESS MEETINGS WITH THE OWNER (ON SITE OR TELECONFERENCE) UNTIL PROJECT COMPLETION.
- 18) PROPER IMPLEMENTATION AND MAINTENANCE OF EROSION CONTROL MEASURES ARE OF PARAMOUNT IMPORTANCE FOR THIS PROJECT. THE CONTRACTOR IS RESPONSIBLE FOR COMPLYING WITH ALL EROSION CONTROL MEASURES SHOWN ON THE PLANS. ADDITIONAL EROSION CONTROL MEASURES SHALL BE INSTALLED IF DEEMED NECESSARY BY ONSITE INSPECTIONS OF THE OWNER, THEIR REPRESENTATIVES, OR STATE/LOCAL/ FEDERAL INSPECTORS AT NO ADDITIONAL COST TO THE OWNER.

LEGEND		
EXISTING	DESCRIPTION	PROPOSED
	GRANITE MONUMENT - 3' OFFSET	
	IRON PIN FOUND/SET	
	IRON ROD FOUND	
	CAPED IRON ROD FOUND	
	DRILL HOLE FOUND	
	GRANITE MONUMENT FOUND	
	STREET LINE	
	LOT SETBACKS	
	PROPERTY LINE	
	ABUTTOR LINE	
	"NO CUT" BUFFER	
	WETLANDS	
	EDGE OF ROAD/TRAVELED WAY	
	SOIL TEST PIT	
	CONTOUR	
	SPOT GRADE	
	GAS SHUT-OFF	
	UTILITY POLE	
	OVERHEAD ELECTRICAL	
	UNDERGROUND ELECTRICAL	
	FIRE HYDRANT	
	WATER LINE	
	WATER GATE	
	SEWER LINE	
	SEWER MANHOLE	
	DRAINAGE MANHOLE	
	CATCH BASIN	
	UNDERDRAIN/STORMDRAIN	
	UNDERDRAIN	
	SILT FENCE	
	TEMP. STONE CHECK DAM	
	GRADING AND FLOW DIRECTION	
	HAY BALES	
	EROSION CONTROL BLANKET	
	STORMWATER BOUNDARY	
	STORMWATER FLOW (To)	
	FACE OF LEDGE OUTCROP	
	BIRCH	
	MAPLE	
	TREE LINE	
	SITE LIGHTING (BAYSIDE FIXTURE)	
	STONE WALL	



WYATT DRIVE SUBDIVISION  
EXISTING CONDITIONS SURVEY

Date:

July 23, 2022

L-1





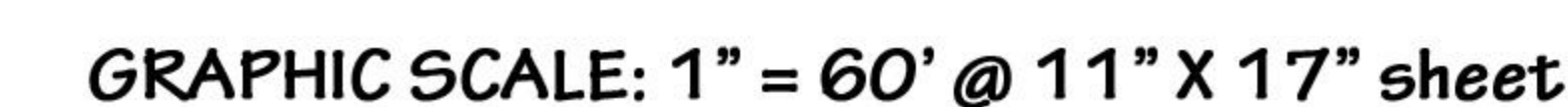
Tom Farmer, Landscape Architect  
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p. 207.749.4032

**WYATT DRIVE SUBDIVISION**

Wyatt Drive  
Cumberland Center, ME

Date: July 26, 2022

**L-2**

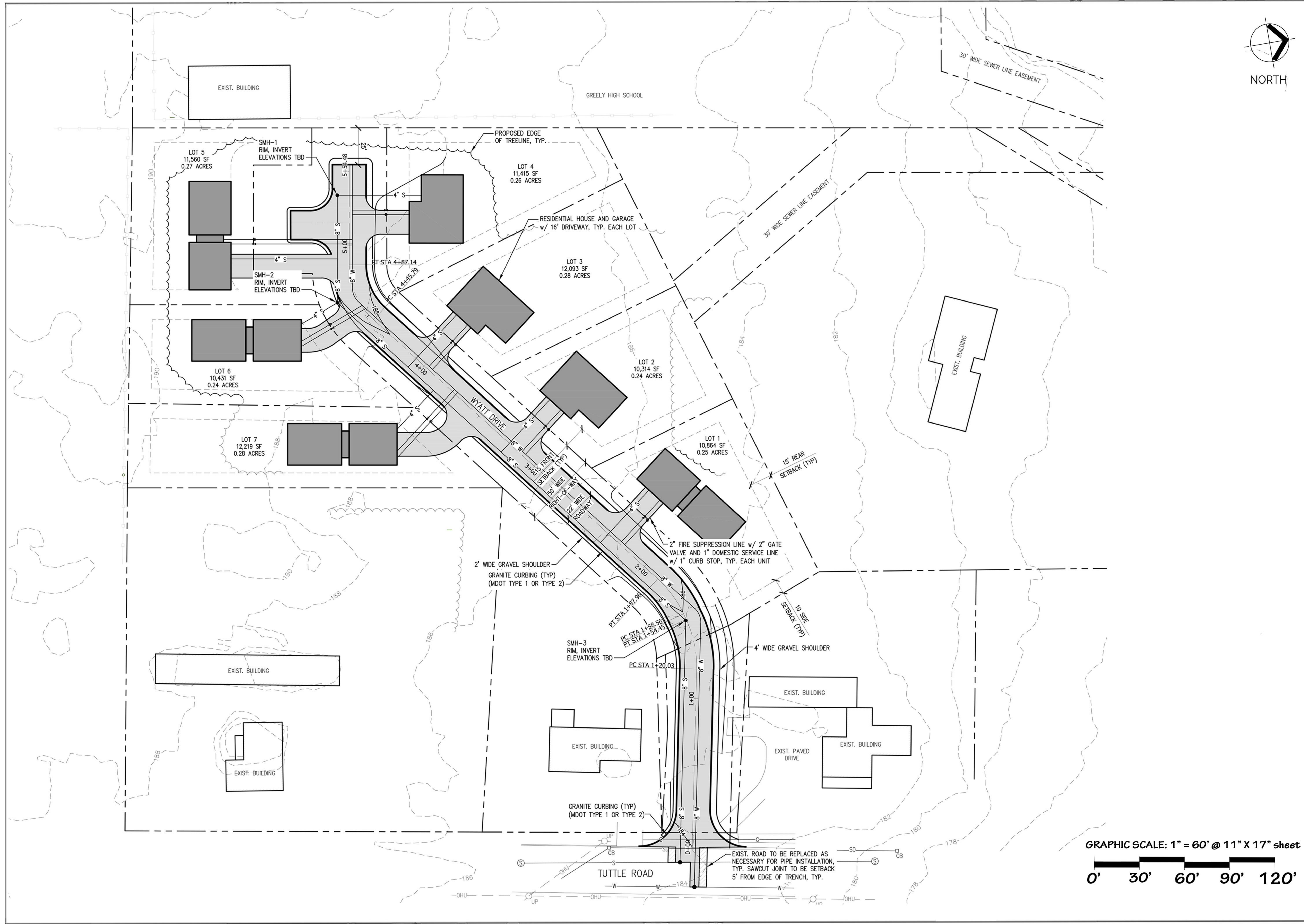






**WYATT DRIVE SUBDIVISION**  
**TRADITIONAL SUBDIVISION DESIGN**  
**(ALTERNATIVE 2)**

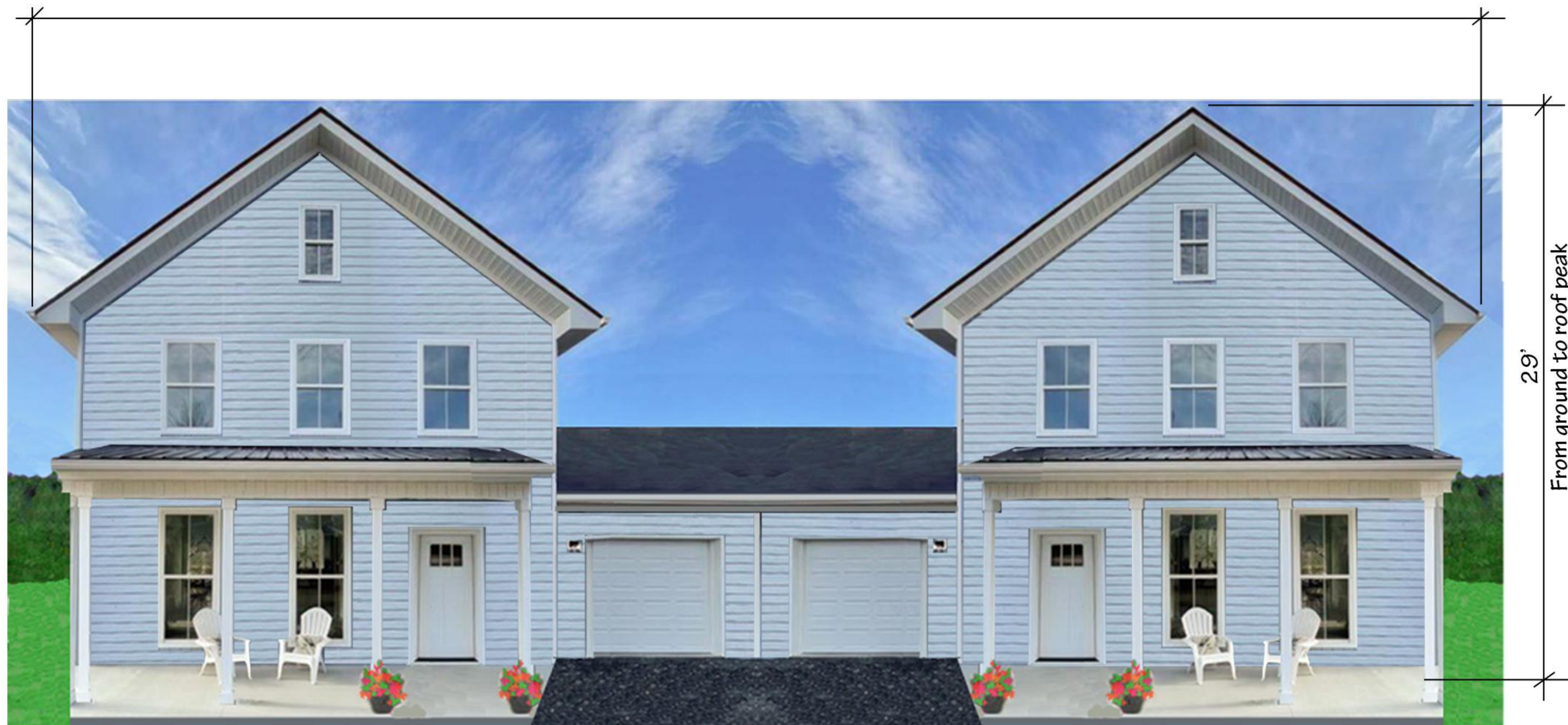
Date:  
July 23, 2022



GRAPHIC SCALE: 1" = 60' @ 11" X 17" sheet







**FRONT ELEVATION**



**NON-GARAGE SIDE ELEVATION**



**REAR and GARAGE SIDE ELEVATION**

**NOTES:**

1. Architectural elevations are illustrative based on real photos of existing, built buildings proposed for this project. Exact scale, color and dimensions may differ slightly based on final product selection and construction.
2. Architectural elevations and floor plans provided by [www.architecturaldesigns.com](http://www.architecturaldesigns.com), Plan #69742AM, "Cozy Two-story Home with Second-level Bedrooms", approx. 1,300 s.f. heated space, 3 bedrooms, 2.5 baths, 2 stories, 1 car garage. See Floor Plans for more details.

**PROPOSED MATERIALS AND COLORS:**

1. SIDING: Vinyl clapboards and trim. Medium to light blue clapboards with white trim.
2. ROOF: Architectural style asphalt shingles, gray black in color.

Architectural elevations are illustrative based on real photos of existing, built buildings proposed for this project. Exact scale, color and dimensions may differ slightly based on final product selection and construction.

**TOM FARMER  
LANDSCAPE  
ARCHITECTURE**

Tom Farmer, Landscape Architect  
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**WYATT DRIVE SUBDIVISION**

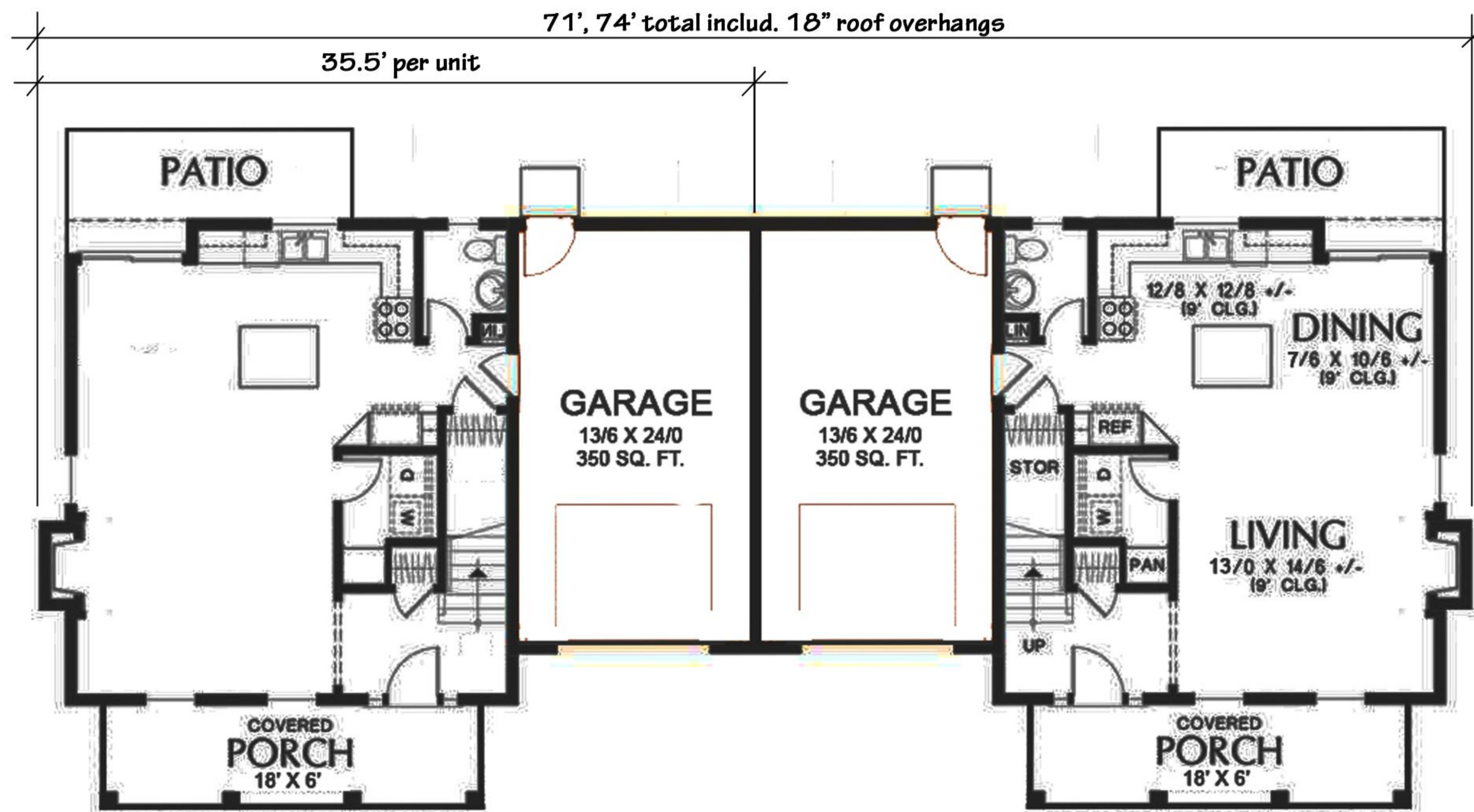
**ARCHITECTURAL ELEVATIONS**

Maine Affordable Properties, LLC.

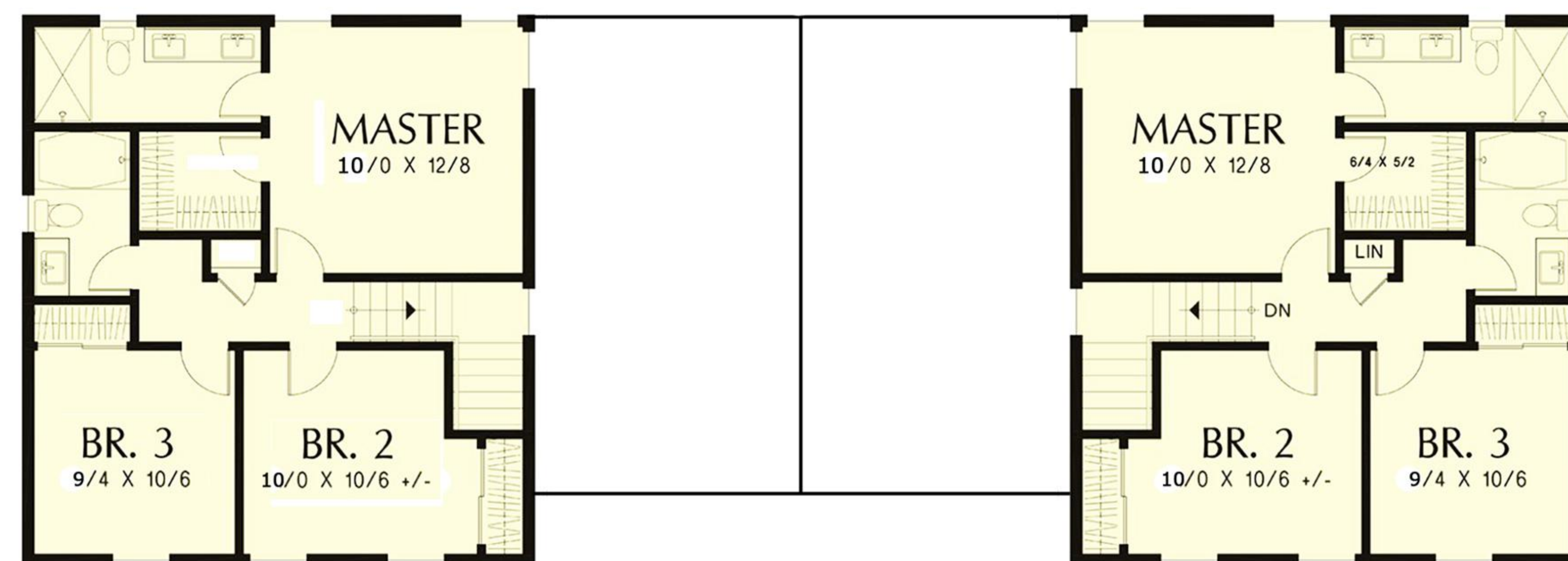
Date:  
07/26/2022

**A-1**





FIRST FLOOR PLAN



SECOND FLOOR PLAN

NOTES:

1. Architectural elevations are illustrative based on real photos of existing, built buildings proposed for this project. Exact scale, color and dimensions may differ slightly based on final product selection and construction.
2. Architectural elevations and floor plans provided by [www.architecturaldesigns.com](http://www.architecturaldesigns.com), Plan #69742AM, "Cozy Two-story Home with Second-level Bedrooms", approx. 1,300 s.f. heated space, 3 bedrooms, 2.5 baths, 2 stories, 1 car garage. See Floor Plans for more details.

TOM FARMER  
LANDSCAPE  
ARCHITECTURE

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WYATT DRIVE SUBDIVISION  
ARCHITECTURAL FLOOR PLANS  
Maine Affordable Properties, LLC.

Date:  
07/26/2022

A-2





2-story home left of entrance



Standing on sidewalk at entrance to site, looking into site



2-story home right of entrance



Wooded buffer along north PL



West buffer & 8' high solid fence (HS) PL



South (cemetery) buffer



East buffer looking toward Dara Bistro and back of apartment bld.



Back yard 367 Tuttle Rd



Exit from site - Cumb Fire Sta.



Sidewalk, looking toward Main St.

Photos taken by  
Tom Farmer, LA,  
July 11, 2022



Village Way Condominiums (VWC)



VWC 18' road, 18' driveway depth



VWC patios 10' from PL



VWC buildings 23' from PL, patios 10'

TOM FARMER  
LANDSCAPE  
ARCHITECTURE

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WYATT DRIVE SUBDIVISION

NEIGHBORHOOD CONTEXT

Maine Affordable Properties, LLC.

Date:  
07/26/2022

P-1